

Fincher Farms

conceptual master plan

12.13.2023

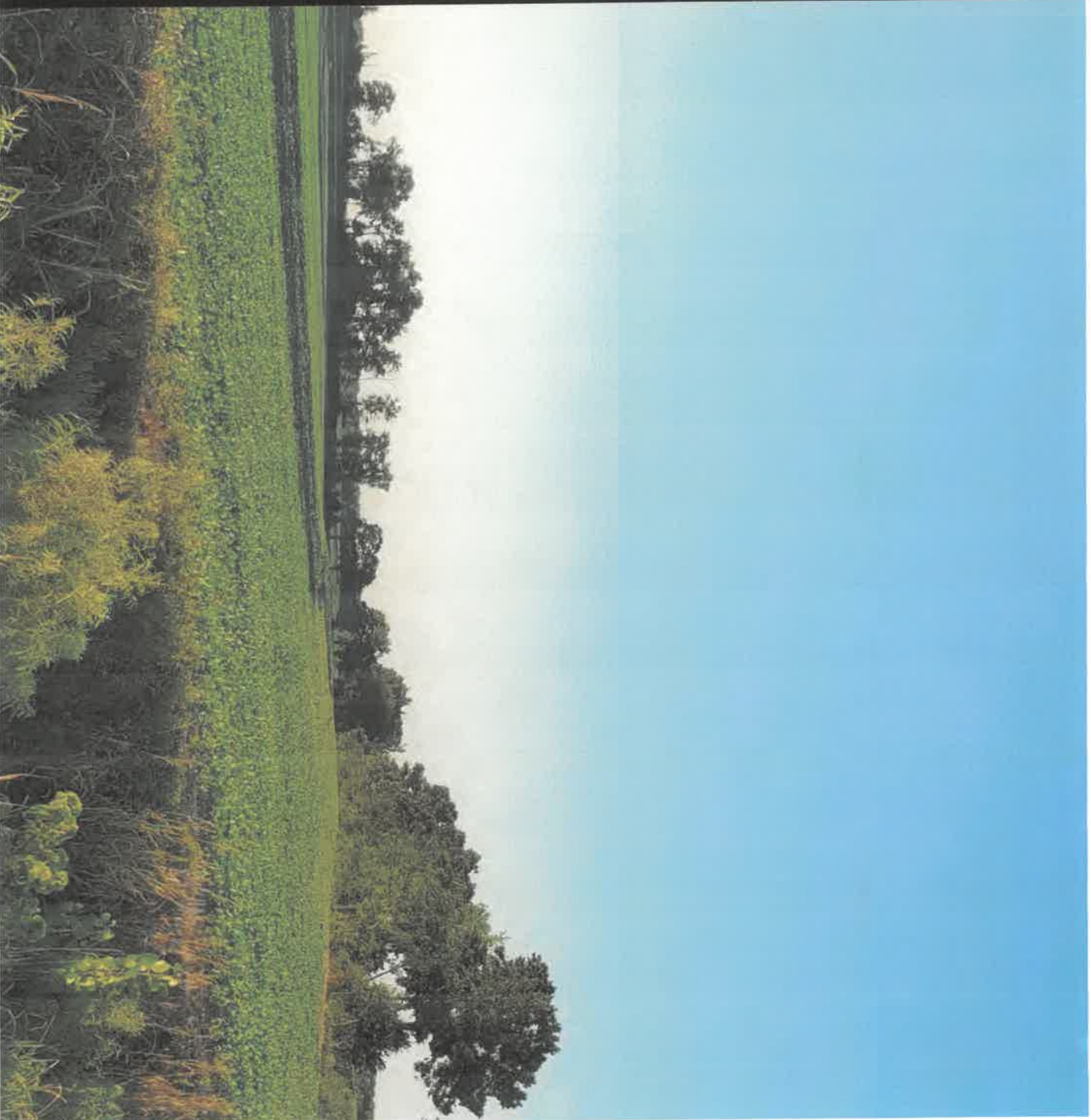


common ground

URBAN DESIGN • PLANNING

Aerial Map of Site





BROWNSVILLE AND HAYWOOD C

INTERIM DEVELOPMI AND DESIGI GUIDELINES

October 18, 2022

Development and Design Guidelines

RESIDENTIAL NEIGHBORHOOD FORM

As a means to support the vision and goals for Haywood County envisioned by its residents, new residential neighborhoods shall incorporate basic principles and approaches to layout and design.

General Design Principles

General Layout — Neighborhoods should be designed at a human-scale with all age groups, and physical abilities in mind. A 5-minute walking radius may serve as a fundamental guide to layout and arrangement of uses, circulation and amenities. Design should reflect a balance between home and building uses, shared amenities, pedestrian connectivity and vehicular access. Neighborhoods defined by lengthy, cul-de-sacs are discouraged in favor of networks of interconnected streets, sidewalks, open spaces and trails. Incorporating future connectivity to adjacent properties should be considered within the design of any proposed development.

Usable Open Space — Neighborhoods should incorporate usable public and/or semipublic open space as highly visible amenities and focal points. Locating significant portions of a neighborhood's usable open space to the rear of homes and residential units or in ways that limit visibility from public views is discouraged.

Mix of Home Types — Large scale development consisting of a mix of home types such as single-family detached homes, townhomes, and flats is encouraged while monolithic neighborhoods defined by only one or two home types are not preferred.

Streets — Streets and their associated right-of-ways should be designed to encourage and support the neighborliness that residents cherish about the community today. Well-designed streets consider a balance between pedestrians, bicycle and vehicular access. Reduced design speeds should be a consideration when designing neighborhood streets.

Refer to Preferred Street Sections and Streetscape Standards (page 24) and refer to Scenic Roads (page 14) where applicable for locating neighborhoods relative to scenic roadways.



Public and semi-public green and open spaces should be incorporated into the neighborhood design as mix of housing types will allow for neighborhoods to offer different types of amenities to people at all stages of life and a broader market.

SITE DESIGN

NEIGHBORHOOD DESIGN

The development plan shall locate buildings, parking areas and open space in an arrangement that promotes the enjoyment of walking, units, buildings, other on-site facilities and the community as a whole by employees, customers and residents of the development.

Walking trails and buildings shall be primarily oriented towards the street and open spaces and away from parking lots and garages.

Active and passive recreational areas and other public and/or semipublic open spaces, such as parks, pocket parks, squares, greens, courtyards, plazas, lawns, pedestrian promenades, community gardens, and dog parks shall be designed to promote use and enjoyment by residents of the development. Such areas shall be designed to utilize natural features of the site, including existing vegetation, where possible, and shall be aesthetically landscaped with a wide variety of plant species. In the event that private property and design ideas should be designed to be architecturally formal and geometrically optical, however, this shall not preclude the use of curvilinear designs for walkways or landscaped areas.

Within new residential and mixed-use developments, usable common open space shall be provided for any development consisting of more than 10 residential units. For a common open space to qualify, the width dimensions of 40 feet and shall be a minimum area of 4,000 square feet. The common open space may be configured as one or more parks, squares, greens, courtyards, plazas, lawns, pedestrian promenades, community gardens and dog parks and may constitute a portion of a larger common open space that will be completed within a subsequent development or phase of development. See Parks and Open Spaces on page 25 for additional recommendations.

The development plan shall incorporate landscaping with other functional and ornamental site design elements, which shall include, but not be limited to, trees, plants, berms and other earth forms, ponds and other water features, accessory structures, fences, walls, furniture as well as art and sculpture.

BLOCKS

The length, width and area of blocks created within a zoning shall be such as to accommodate the lot sizes and dimensions required for the specific use(s) and to provide for convenient access, circulation, and accommodation of permitted uses as defined in the application.

In general, the unobstructed length of interior blocks and streets shall not exceed 450 ft. The walking distance between two homes located on each side of a not exceed 900 ft. Exceptions may be considered subject to site specific design.

LOTS

Not lot shall be created on which development may be rendered impracticable due to significant environmental or non-typical constraints, such as steep slopes, wetlands, floodplains, flood hazard areas, drainage or other natural conditions or lack of sewer capacity, utility service, vehicular access or other non-typical conditions; lots which front upon open space and not a street shall have its vehicular access from an alley.

LOT LINES

Side lot lines shall be at right angles to straight street lines and radial to curved street lines, wherever practical.

- Provide pedestrian and bicycle paths and trails
- Provide open spaces and parks
- Provide streets and sidewalks
- Orient streets and sidewalks
- Provide open spaces and parks
- Provide streets and sidewalks
- Provide pedestrian and bicycle paths and trails

Development and Design Guidelines

COMMUNITY

MULTIFAMILY

The incorporation of multi-family and senior living home options will better ensure a wide variety of housing types are available within the community. To better ensure the character and form of multi-family and senior living development, embodies the values and goals of the community, the scale, character and qualities of new development should be carefully considered. By incorporating multi-family and senior living opportunities as integral components of Hayward County's growth, these investments will serve to help the community achieve stated goals and vision.



Buildings engage the public realm and help define the character of adjoining walkable streets. Both ground floor porches, stoops, porches and stoop entrances are directly accessed from the sidewalk. Some parking is accommodated via on-street parking.

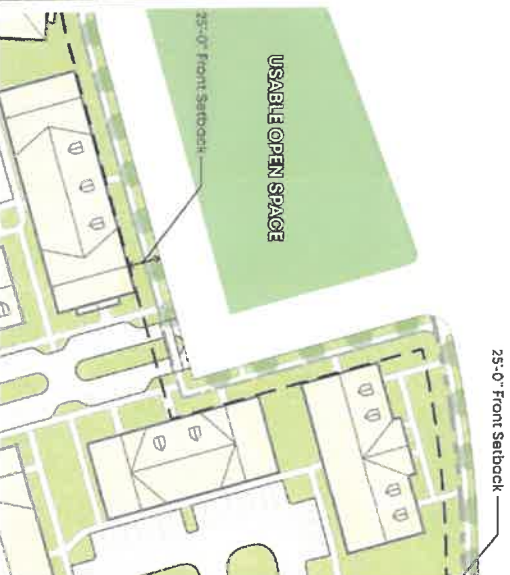


Multi-family homes come in a variety of forms which appeal to different lifestyles and market segments. The units above include reserved parking and a mix of living space, either to a townhome variety in style and amenities are encouraged. Note that ingressed, tucked under parking should NOT face a public street.

22 BROWNSVILLE AND HAYWOOD COUNTY



The building addresses the neighborhood green and porches and stoops provide direct access for residents to the park space. Dormers, setbacks, porches and shutters are appropriately sized and scaled. Highly visible and usable open space is a critical feature of this approach.



Buildings and homes face, engage with and form both public roads and private drives including pedestrian scales, walkable streetscapes, off-street parking, and other amenities. A variety of adjoining uses may line the street resulting in a vibrant, mixed-use neighborhood.

GENERAL DESIGN STANDARDS

LANDSCAPING AND SCREENING

Front suitability, maintenance and compatibility with site and construction features are critical factors which shall be considered. Plants shall be non-toxic, non-invasive and drought-tolerant and to the best possible extent, native.

All ground level HVAC equipment and utility service boxes shall be suitably buffered and screened to minimize views of the same from both within the site itself, as well as from adjacent and nearby properties and public right-of-way.

Where a necessary wall or solid wooden fence is utilized, the design of such shall be architecturally compatible with the style, material, color and details of the building(s) on the site. Chain link fencing is not permitted.

Fencing located between the building and street is strongly discouraged. Where fencing is located between the building and the street, fence design recommendations located on page 23 are applicable.

Where an evergreen screen is utilized, such plantings shall be in a continuous row and may be overlapped or staggered within such row.

LIGHTING

Well-designed soft lighting of the building exterior shall be permitted, provided that the lighting complements the architecture. The lighting shall not draw interior attention to the building.

For all mixed-use and multi-family lots, all areas between buildings, along walkways, active recreation areas, driveways, parking lots and loading areas shall be adequately illuminated to ensure security and safe pedestrian and vehicular circulation.

See recommended lighting standards, page 35.

OPEN SPACE, AMENITIES, & BUFFERS

Attached dwelling developments shall provide extensive sidewalk and trail systems linking to surrounding street networks, greenways, and public parks.

To provide informal surveillance, a sense of safety, and encourage a sense of ownership of amenities and open spaces by residents of the development, porches, entries, and windows of surrounding buildings shall be designed to orient towards streets and usable open space.

Streetview elements (e.g. benches, trash receptacles, light fixtures, bollards, mail boxes, fountain, bicycle racks, etc.) includes within an attached or multi-family residence development shall be compatible with the architectural features of the structures, and shall help to establish a unifying theme throughout the site.

Landscape buffers are encouraged at the side and rear of sites.

PARKING, CIRCULATION, AND CONNECTIVITY

Garage, carport and parking lots should not face a public or primary roads and must be hidden from view of the public realm and primary frontages by their location on site, preferably, and by opaque screening.

Locate parking within close proximity to the building it is intended to serve. Where a building is situated along a public street, a minimum of one shared entrance shall be provided.

Site design and layout shall take into consideration the creation of safe pedestrian paths for children to reach nearby schools and/or bus stops (existing or proposed). Site design and layout shall incorporate safe and comfortable informal pedestrian circulation (e.g. paths) to access all private streets and services.

Refer to general parking principles on page 45.

- Include multi-family and senior living
- Orient buildings to public streets
- Open up spaces for public use
- Orient buildings to public streets
- Park and walkable streets
- Build high-quality, walkable streets
- Screen parking areas

Development and Design Guidelines

COMMUNITY

COMMERCIAL

As the residential population and the number of people employed in Haywood County increases, the need and potential for new retail, service and business activity will increase as well. This section of the guidelines informs commercial development patterns and design in anticipation of new investment and reuse of underutilized properties. In support of the resident informed vision and goals for Haywood County, retail and commercial development should occur at appropriate scales, embody both traditional and contemporary character, and create an attractive and walkable environment that reflects and connects with the surrounding community.

General Design Principles

General Considerations – Design should consist of well scaled buildings which build upon the community's architectural traditions while incorporating modern design innovation where appropriate. Buildings should be defined by tall interior spaces with ample transparency along streets and sidewalks. Entrances to all buildings should be clearly established with elevated consideration given to pedestrian access via wide sidewalks. A pattern that establishes internal streets and place making should be given priority within large developments. For signage recommendations see page 35.

Usable Open Space – Commercial development should incorporate usable open space into layout and design. Such open space may consist of wide sidewalks where outdoor dining and retail may occur, plazas for live music and events, and open spaces for informal recreation meant to attract and retain customers.

Mix of Uses – Incorporating hospitality, office, retail, and medical uses into an integrated framework of walkable streets and activated spaces results in a more resilient and economically sustainable development pattern while providing a broad range of services and appeal to area residents. Developments defined by buildings and improvements isolated from neighboring uses are discouraged.

Off-Street Parking – Large, open parking lots will be minimized with parking for peak periods accommodated to the side and rear of buildings. Large parking lots should incorporate improved design and circulation principles with internal landscaped drives, sidewalks, and landscape islands serving to divide oversized lots into a series of parking "rooms". For additional parking lot recommendations, see page 45.



Left photograph shows a building with a covered outdoor seating area. Right photograph shows a street scene with people walking and a building.



The commercial space supports a simple, scale, and form of design defined through large spaces for outdoor dining and adjacent building materials and improved open space.



The development pattern above depicts retail and commercial uses benefiting from a visibility along the primary street, and designed around pedestrian scaled streets and open lot use in a way that creates a true destination.

GENERAL PRINCIPLES AND SITE DESIGN

SUMMARY

Buildings shall have architectural features and patterns that create visual interest, reduce large areas of unadorned building facade, and shall include a rhythm incorporating a variety of elements and masses. Design shall consist of well scaled buildings which build upon the community's architectural traditions while incorporating modern design innovations where appropriate.

DIVERSITY

Create places that support a diverse range of employment types, retail, office and other services.

VARIETY

Vary building scale, densities, and rhythm to provide visual interest while enabling design that builds on local architectural traditions, responds to surrounding context and encourages innovation.

ENHANCE THE PUBLIC REALM

A hierarchy and network of public and semi-public gathering spaces should be carefully located, sized and interconnected to provide a range of features, activities, and programmable event space for public enjoyment and amenity for customers, employees and businesses. Features that enhance the public realm include space and programming for outdoor cafes, outdoor seating, benches, informal spaces for live music, informal family-friendly lawn games such as corn-holes, and other related street-scapes elements and uses.

Where possible, incorporate a wide range of arts related uses, arts initiatives and cultural programming.

Provide highly visible public gathering spaces and a sense of place in the heart of the community.

Pole mounted signs are discouraged in all locations.

OUTPARCELS

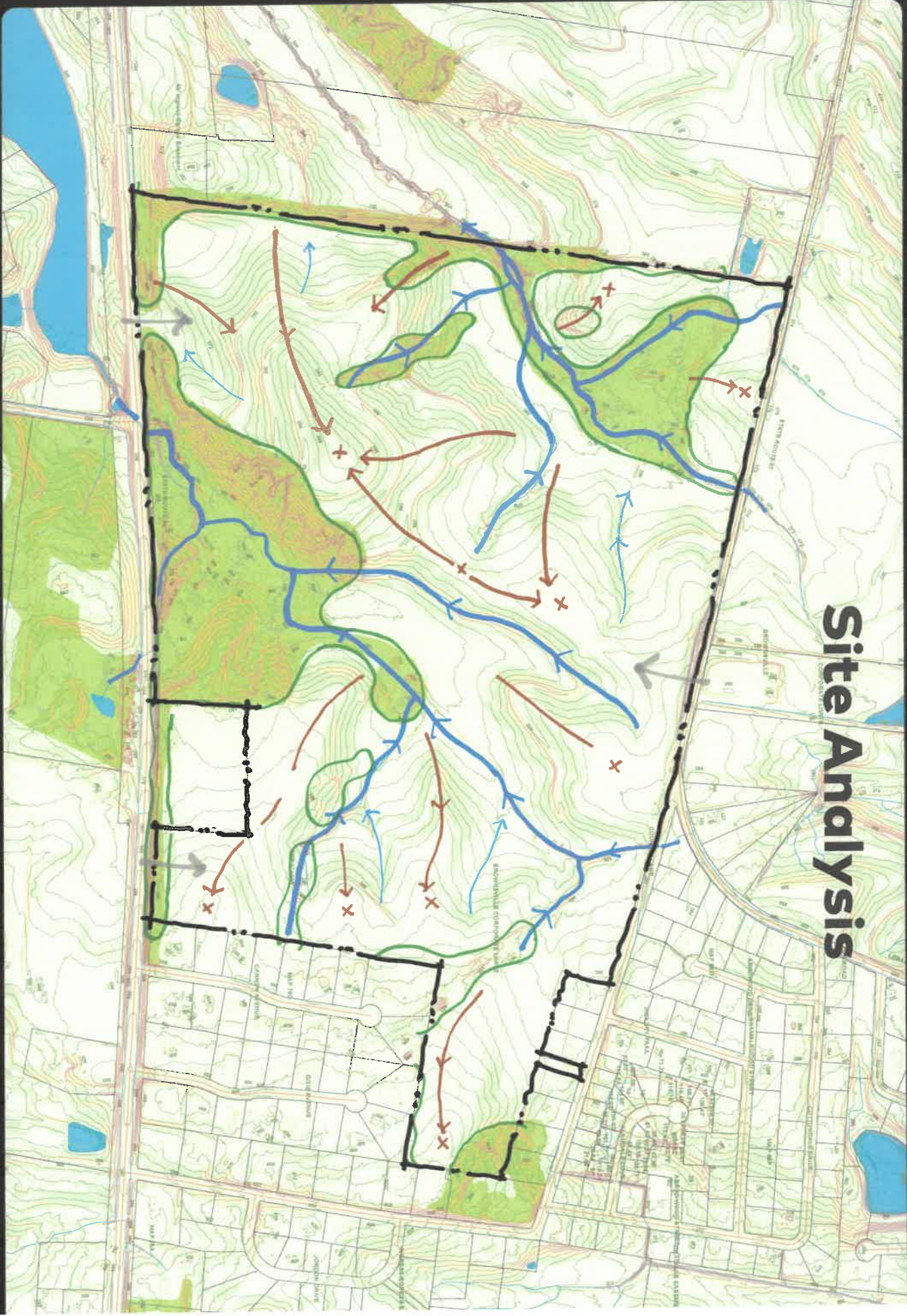
Outparcel buildings should be used to frame corners, define street edges, and orient traffic toward primary and secondary entrances from public right-of-way.



Buildings address and engage the informal open space via transparency, entrance design and orientation, and outdoor seating.

- Incorporates
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Site Analysis



Conceptual Master Plan

- KEY**
- 1 Village Green
 - 2 Plaza with Food Truck Access
 - 3 Civic Building
 - 4 Neighborhood Green
 - 5 Water Feature (SWM)
 - 6 Enhanced Stream Bed
 - 7 Preserved Mature Tree Masses
 - 8 Natural/Landscaped Buffer
 - 9 Potential Civic Amenity
 - 10 Village Center
 - 11 Access to Water
 - 12 Courtyard
 - 13 Potential Park and Ride
 - 14 Mid-Block Pedestrian Passages



CONCEPTUAL DEVELOPMENT	
LOT TYPES	LOT/G
Commercial/Mixed-Use	5.1/
Multi-family (Rear Loaded)	17.4/
Townhouses (Rear Loaded)	9.4/
Two-Family Houses (Front Loaded)	5.0/
Single-Family Detached Cottages (Rear Loaded)	4.0/
Single-Family Detached Houses (Front Loaded)*	5.0/
Micro Houses	3.0/
Total:	

*Lots fronting Country Lane or Fulton Road will be 10,000

Mixed-Use/Commercial Precedent Image



Multi-Family Flats Precedent Image



Townhouse Precedent Imagery



Two-Family House Precedent Image



Micro Homes Neighborhood Precedent In



Cottage Precedent Imagery



House Precedent Imagery



Driveway narrows in width at street with sidewalk material running continuous.

Garage doors are recessed.

HVAC units and utility meters are properly screened from public view.



Building form, with wrap-around porch, offset garage as a secondary element, etc. responds to corner and double street frontage.

Front walkway addresses the primary street and terminates at sidewalk.

Building is properly located relative to the front setback line.

In some cases, it may be appropriate to provide a sense of enclosure to courtyards with garden walls or landscaping.

Auto court garages should appear detached from the primary house with their own architectural language. The architectural style, materials and details of the structure will be in keeping with the primary house. Garage doors must be carriage style doors. Typical metal panel doors are not allowed.

Homes exceeding 50% coverage of a residential lot may be appropriate in neighborhoods of exceptional design as outlined in this document.

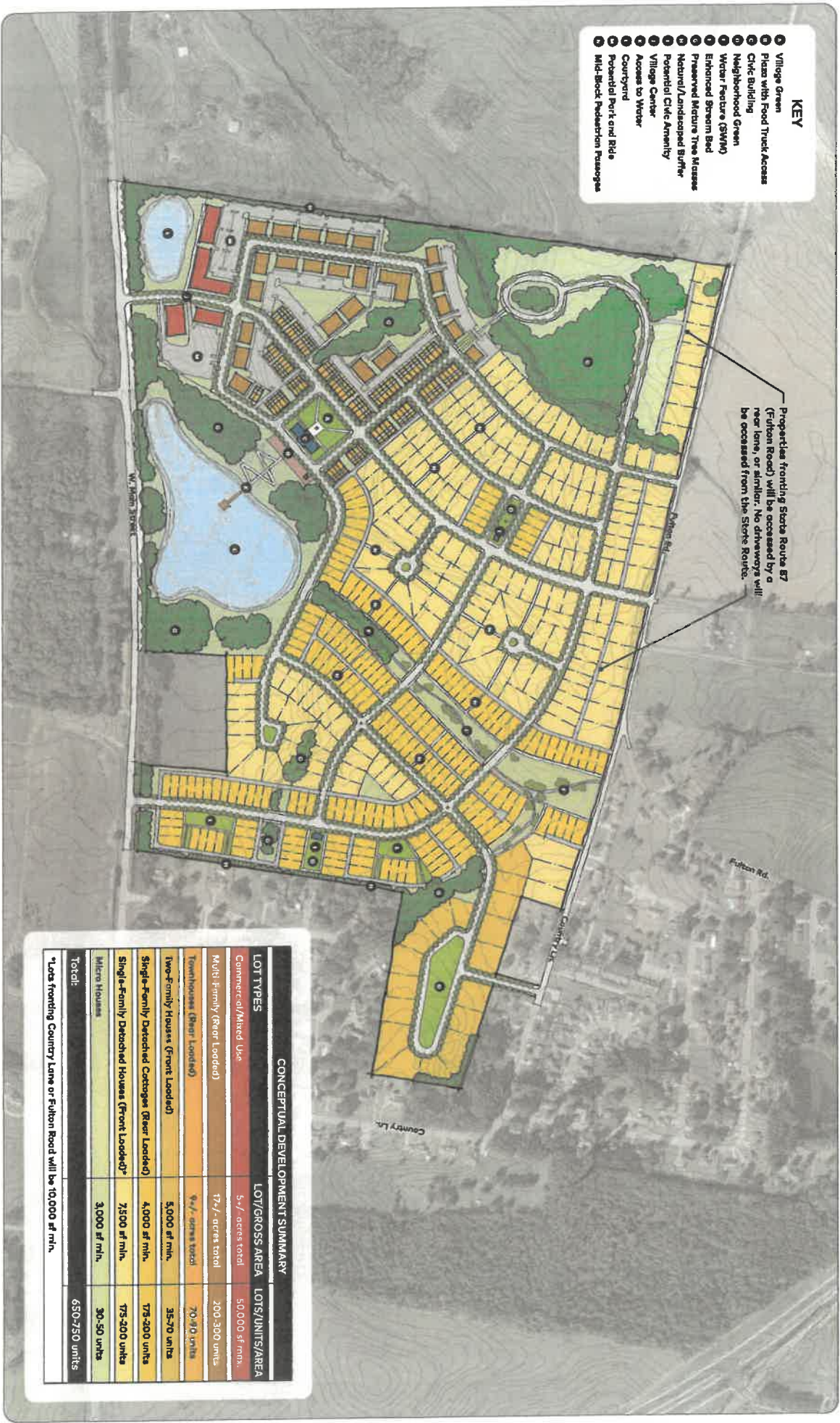


Civic/Amenity Precedent Imagery



- KEY**
- 1 Village Green
 - 2 Plaza with Food Truck Access
 - 3 Civic Building
 - 4 Neighborhood Green
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 - 6 Enhanced Stream Bed
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 - 14 Mid-Block Pedestrian Passages

Properties fronting State Route 67 (Fulton Road) will be accessed by a rear lane, or similar. No driveways will be accessed from the State Route.



CONCEPTUAL DEVELOPMENT SUMMARY

LOT TYPES	LOT/GROSS AREA	LOTS/UNITS/AREA
Commercial/Mixed Use	5 +/- acres total	50,000 sf max.
Multi-Family (Rear Loaded)	17 +/- acres total	200-300 units
Townhouses (Rear Loaded)	4 +/- acres total	70-80 units
Two-Family Houses (Front Loaded)	5,000 sf min.	35-70 units
Single-Family Detached Cottages (Rear Loaded)	4,000 sf min.	175-200 units
Single-Family Detached Houses (Front Loaded)*	7,500 sf min.	175-200 units
Mixed Houses	3,000 sf min.	30-50 units
Total:		650-750 units

*Lots fronting Country Lane or Fulton Road will be 10,000 sf min.

Finchler Property • CONCEPTUAL ILLUSTRATIVE PLAN

01/17/2024





Fincher Farms • STREET TYPE DIAGRAM

01.05.2024

0 50' 100'

common
GEOGRAPHIC



- CS-60-36
- NS-60-36
- NS-50-26
- Sq-Varies-Varies
- Gr-Varies-Varies
- RA-20-16

Fincher Farms • STREET TYPE DIAGRAM

01.05.2024